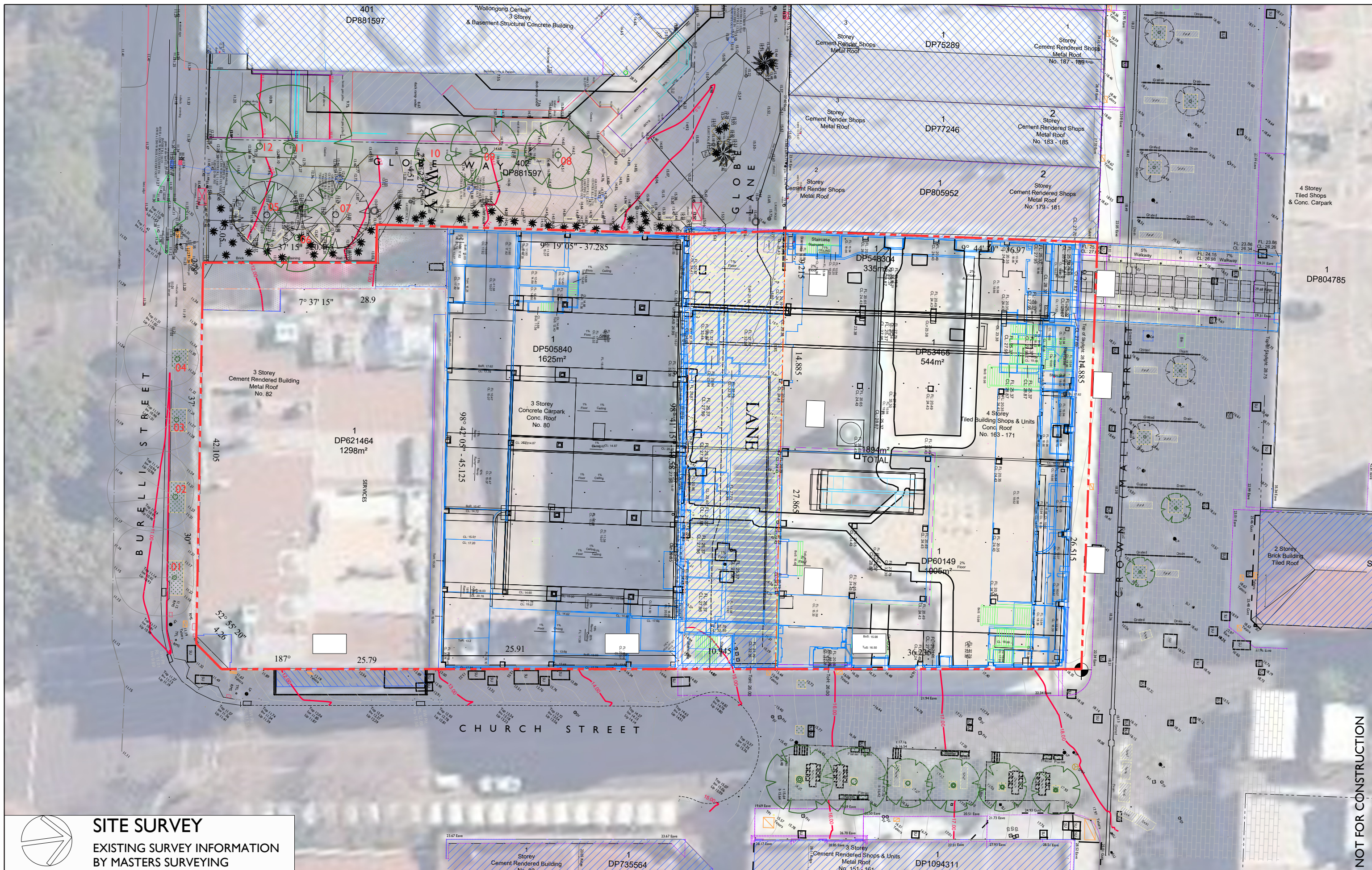
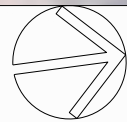


Attachment 1 – Plans



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SITE SURVEY

EXISTING SURVEY INFORMATION BY MASTERS SURVEYING

LEGEND



SUBJECT SITE

B	07-01-25	REISSUED FOR DA		
A	18-10-24	ISSUED FOR DA		
ISSUE	DATE	DESCRIPTION	initials	chkd

sign _____ date _____
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CO-ORDINATION OF WORKS - These drawings are to be read in conjunction with the full set of consultant's drawings, specifications, reports and DA conditions.

DESIGNER - The Designer for this project is ADM Architects Pty Ltd (ACN 609 490 490) (Australia) Pty Ltd T/AS ADM Architects is Angelo Di Martino AR No 7608



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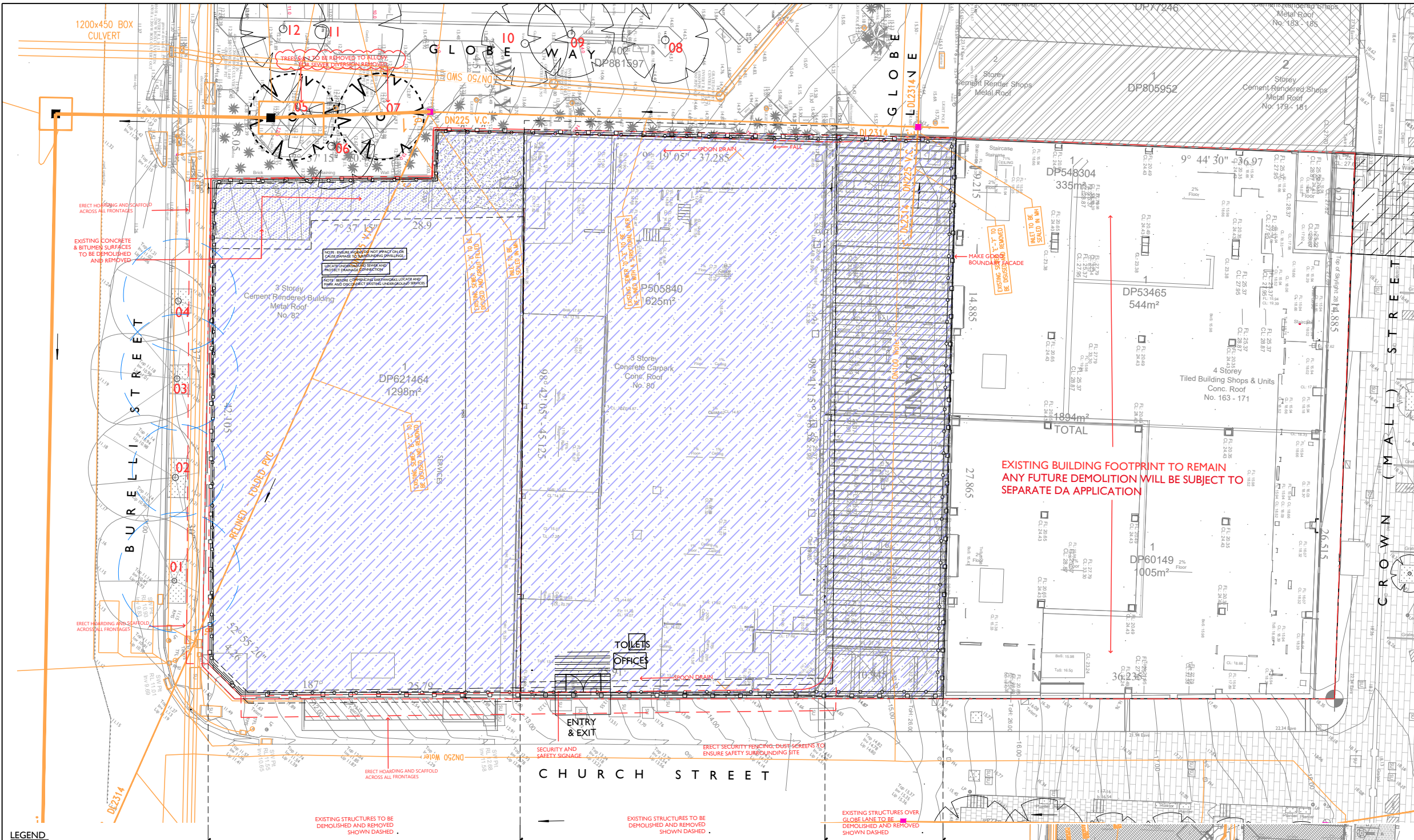
Nominated Architects
Dennis Barker (B100), John Goss (E290), Daniel Hutton (B115)

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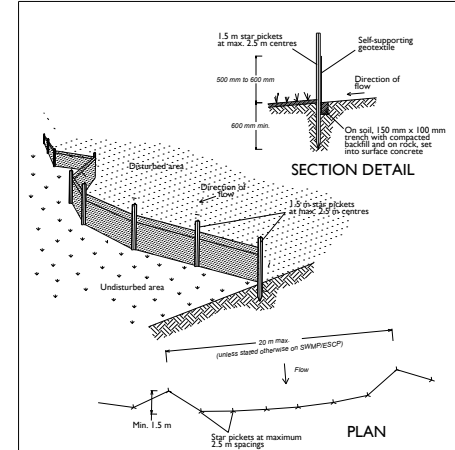
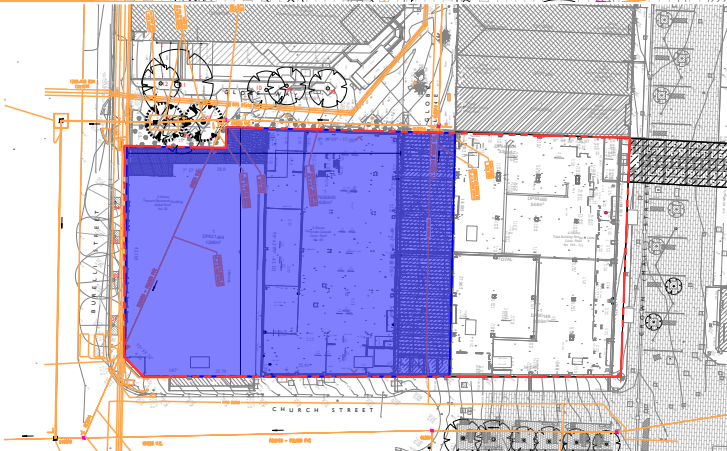
Project
PROPOSED 3-STAGE MIXED USE
DEVELOPMENT COMPRISING OF
HOTEL, OFFICE, MOTEL AND CLUB
OVER BASEMENT PARKING
for
171 CROWN PTY LTD
at
80-82 CHURCH & 163-171 CROWN ST
WOLLONGONG

scale	1:200 @ A1	1:400 @ A3
date	JANUARY 2025	
drawn	HR CA	chkd ADM
drawing	DEVELOPMENT APPLICATION EXISTING SITE SURVEY	
Project No.	Drawing No.	
2021-15A	A-001	

issue
B



- LEGEND**
- SEDIMENT FENCE
 - SAFETY SECURITY FENCE CONTRACTORS COMPOUND
 - HARD STAND AREA
 - TO BE DEMOLISHED & REMOVED
 - TREE TO BE REMOVED
 - TPZ
 - Sewer Deviation



- SEDIMENT FENCE NOTES**
- Construct sediment fences as close as possible to being parallel to the contours of the site, but with small returns as shown in the drawing to limit the catchment area of any one section. The catchment area should be small enough to limit water flow if concentrated at one point to 50 litres per second in the design storm event, usually the 10-year event.
 - Cut a 150mm deep trench along the upslope line of the fence for the bottom of the fabric to be entrenched.
 - Drive 1.5 metre long star pickets into ground at 2.5 metre intervals (max) at the downslope edge of the trench. Ensure any star pickets are fitted with safety caps.
 - Fix self-supporting geotextile to the upslope side of the posts ensuring it goes to the base of the trench. Fix the geotextile with wire ties or as recommended by the manufacturer. Only use geotextile specifically produced for sediment fencing. The use of shade cloth for this purpose is not satisfactory.
 - Join sections of fabric as a support post with a 150mm overlap.
 - Backfill the trench over the base of the fabric and compact it thoroughly over the geotextile.

NOT FOR CONSTRUCTION

DEMOLITION & SITE MANAGEMENT PLAN

B		07-01-25		REISSUED FOR DA							
A		18-10-24		ISSUED FOR DA							
ISSUE		DATE		DESCRIPTION				initials		chkd	
sign				date							
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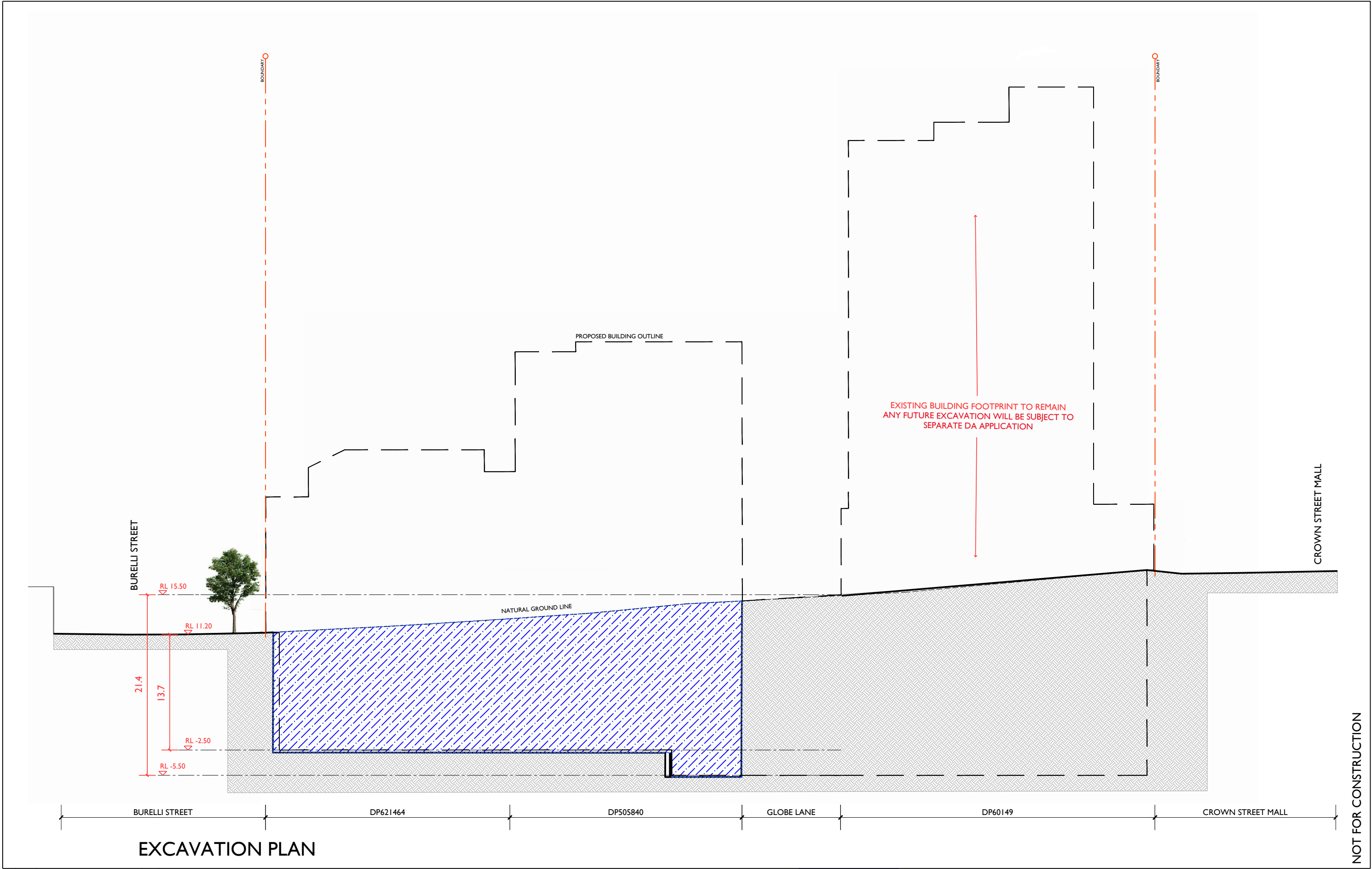
NOMINATED ARCHITECT - The nominated Architect for ADM Projects (Australia) Pty Ltd T/A ADM Architects is Angelo Di Martino ARB No.7608



JACKSON TEECE

171 CROWN PTY LTD
80-82 CHURCH & 163-171 CROWN ST
WOLLONGONG

Project	PROPOSED 3-STAGE MIXED USE DEVELOPMENT COMPRISING OF HOTEL, OFFICE, MOTEL AND CLUB OVER BASEMENT PARKING
scale	NTS
date	JANUARY 2025
drawn	HR CA chkd ADM
drawing	DEVELOPMENT APPLICATION
Project No.	2021-15A
Drawing No.	A-002
issue	B



EXCAVATION PLAN

NOT FOR CONSTRUCTION

A	18-10-24	ISSUED FOR DA		
ISSUE	DATE	DESCRIPTION	initials	chkd
sign			date	
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Project
PROPOSED 3-STAGE MIXED USE DEVELOPMENT COMPRISING OF HOTEL, OFFICE, MOTEL AND CLUB OVER BASEMENT PARKING
for
171 CROWN PTY LTD
at
80-82 CHURCH & 163-171 CROWN ST WOLLONGONG

scale	1:200 @ A1	1:400 @ A3
date	OCTOBER 2024	
drawn	HR CA	chkd ADM
drawing	DEVELOPMENT APPLICATION EXCAVATION PLAN	
Project No.	2021-15A	Drawing No. A-003
issue	A	



BUILDHIGH

Construction Traffic Management Plan (Globe Project–Demolition and Excavation) – Rev 0

80-82 Church St Wollongong

1. Site Background Information

Existing traffic conditions

The proposed development site is located at 80 and 82 Church Street, Wollongong. It is located within the commercial centre of Wollongong. The area of the proposed development is bound by Globe Lane to the north, Church Street to the east, Burelli Street to the south and Globe Way to the west.



Figure 1 – Site Frontages

Church St is a section of road with a cul-de-sac to the northern end that is generally used as a drop off zone for pedestrians, provides access to Globe Lane underpass for registered access users and is also used as a taxi rank. It is also used for servicing some bins to Crown St Mall businesses. The majority of construction activities are proposed to be carried out from Church St. It is proposed to have the existing loading zone amended to a 30m work zone to allow for construction activities to be undertaken including truck parking for demolition and excavation works, deliveries including concrete trucks, steel and formwork trucks. The remaining zone within the Church St frontage is proposed to remain as a loading zone for local businesses. It is proposed to have an A Class Hoarding constructed to the western side of Church St and closed to pedestrian access. Pedestrian access will



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be maintained on the eastern side of Church St to provide a link between Burelli St and the Crown St Mall precinct.

Globe Lane is an existing section of road that is used for local business temporary parking, deliveries, bin storage and Wollongong Council Storage Area (Market Stalls). It is proposed to have this section of road closed during the demolition phase to allow for the safe demolition of the section of building over this laneway. The consultation has been carried out for this and application made to council for a closure for a 7-month period. Alternative storage bin storage areas are to be provided by the builder and a storage area has been offered to Council for the temporary storage of market stall equipment during the road closure.

Burelli St is a main throughfare road that includes the local bus route. It is proposed to minimise the impact on this road during site activities due to the site proximity to the traffic lights and the existing street trees impacting the availability of the road frontage. Burelli St frontage is proposed to have a B Class hoarding installed to allow safe pedestrian movement across the full extent of the frontage and allow pedestrians to cross at the Church St traffic light intersection. It is also proposed to add an additional loading zone within Burelli St to service larger truck deliveries for Globe Lane businesses that are affected by the proposed temporary closure of Globe underpass.

Globe Way is a pedestrian throughfare to access Globe Lane that is managed by Wollongong Council Property Management Division. It is proposed to install an A Class Hoarding to the full extent of the site along Globe Way to secure the site from the public and maintain safety.



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General site access and egress – Demolition and Excavation

Site Access and Egress – Demolition Phase

During the demolition phase Site Entry and Access point 1 will be used for the first part of the demolition whilst the existing section of the David Jones building that is over Globe Lane is be demolished. Trucks will enter north on Church St and Enter and exit under Globe Lane under traffic control guidance.

An alternative site entry and exit point is to be established for deliveries of scaffold and similar materials at 82 Church St Wollongong to keep the roadway clear.

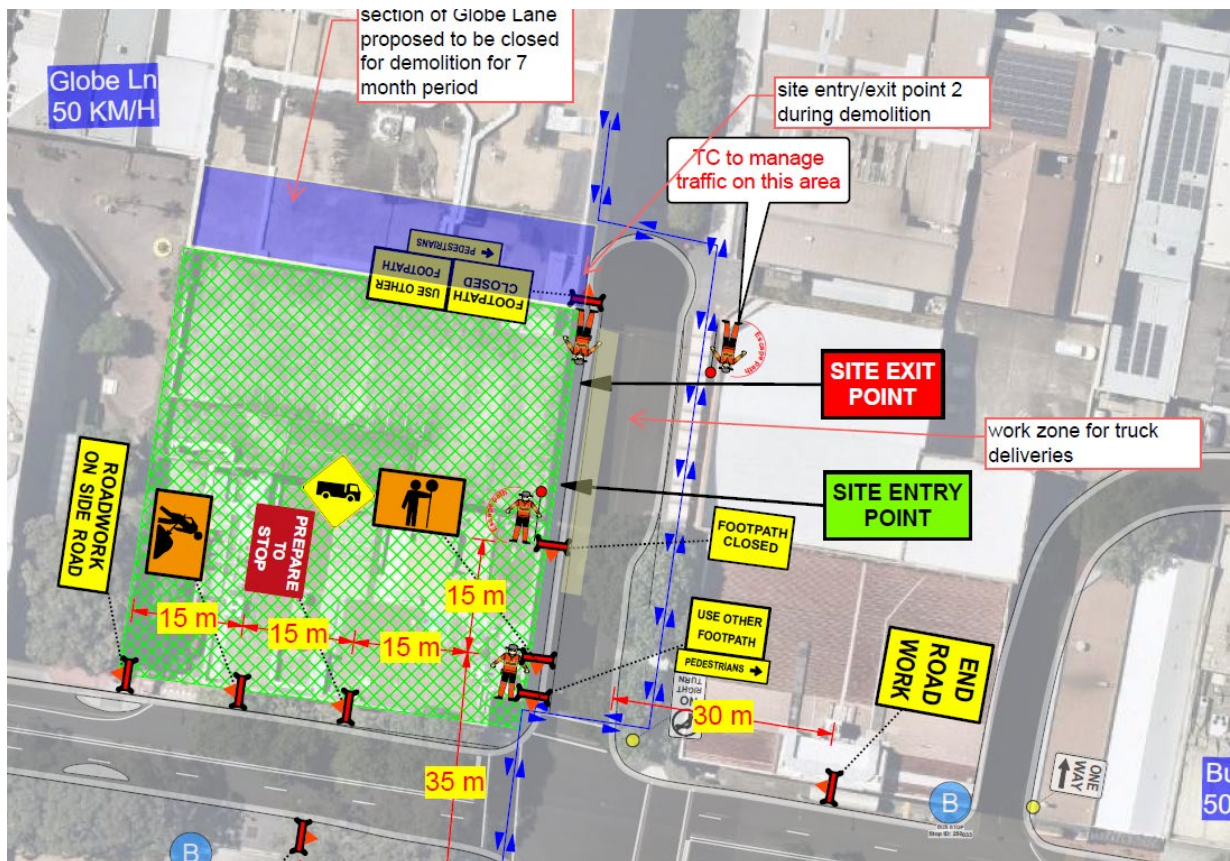


Figure 2 – Site Access – Demolition Phase

Site Access and Egress – Excavation Phase

During the excavation phase the site entry and access point will be at 82 Church St Wollongong only.

Trucks will enter north on Church St and park within the builder works zone area for loading. Once loaded the trucks will turn around in the cul-de-sac under traffic control guidance and exit south along Church St. Trucks will be sized to suit this load out requirement.

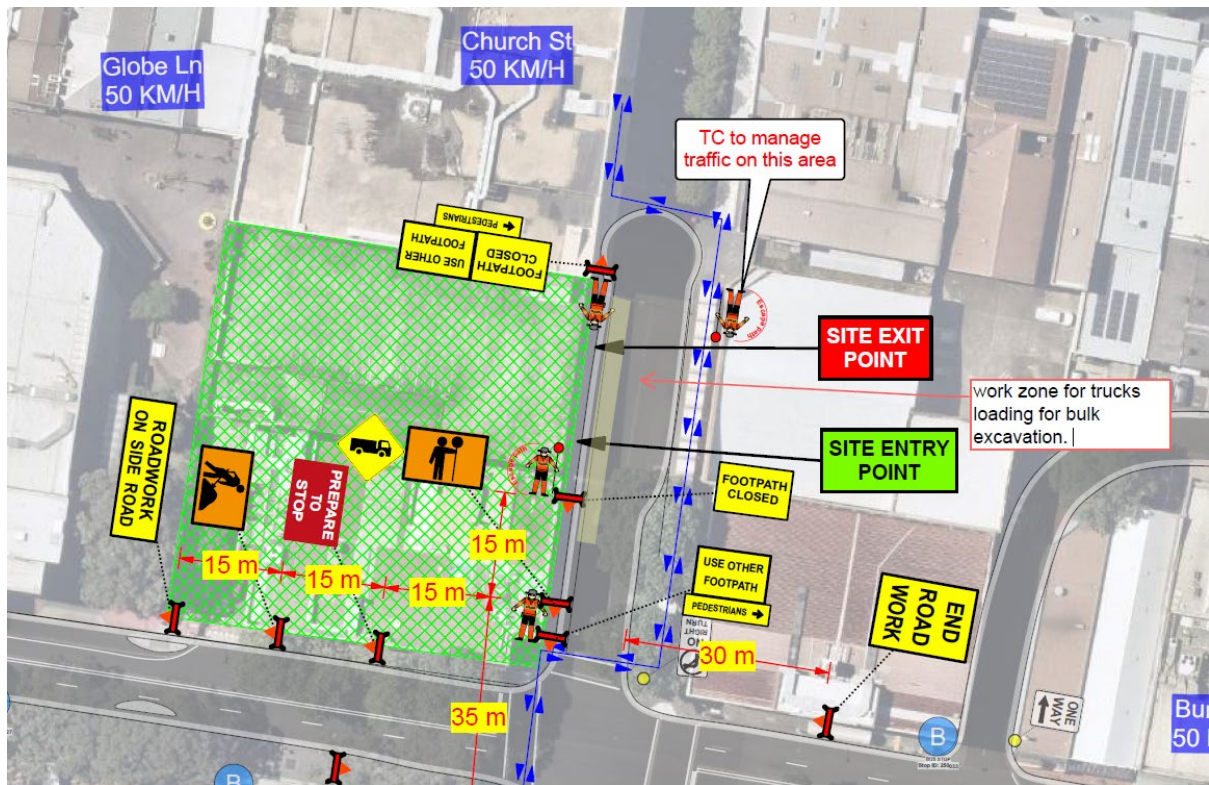


Figure 3 – Site Access – Excavation Phase

Assessment of existing public domain and pedestrian areas

Initial consultation has been carried out with Wollongong City Council regarding the existing public domain and pedestrian areas.

During the demolition phase the pedestrian access to the Globe Lane undercroft area will be removed as the road closure is required to allow for the safe demolition of the section of building over. The establishment of a B Class Hoarding on Burelli St will allow for pedestrian movement from Burelli St, across the traffic lights at Church St and the eastern side of Church St to the Crown St Mall precinct. During the demolition phase the pedestrian movement through Globe Way will also be maintained.

Pedestrian movement to the western side of Church St will need to be suspended for the duration of the demolition and excavation works to maintain public safety due to the high amount of truck movements and construction activity working over.

Refer to the pedestrian movement areas as marked in Yellow in Figure 4 Below.

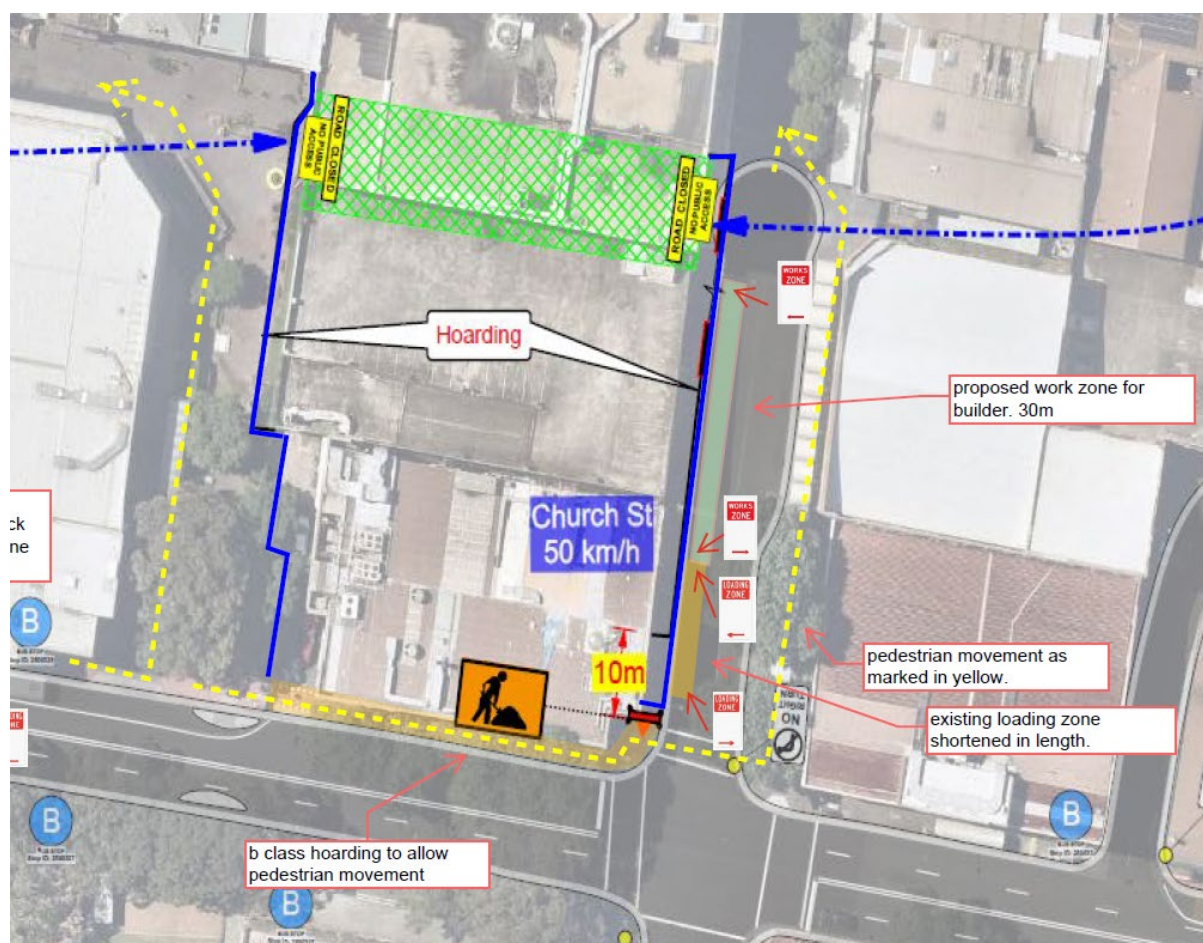


Figure 4 – Pedestrian Movement – Demolition Phase

After the demolition phase the pedestrian movement will be re established to the Globe Lane section of road that had to be temporarily closed.

Pedestrian movement to the western side of Church St will need to be suspended for the duration of the excavation works to maintain public safety due to the high amount of truck movements and construction activity working over.

Refer to the pedestrian movement areas as marked in Figure 5 below for the duration of the excavation works.

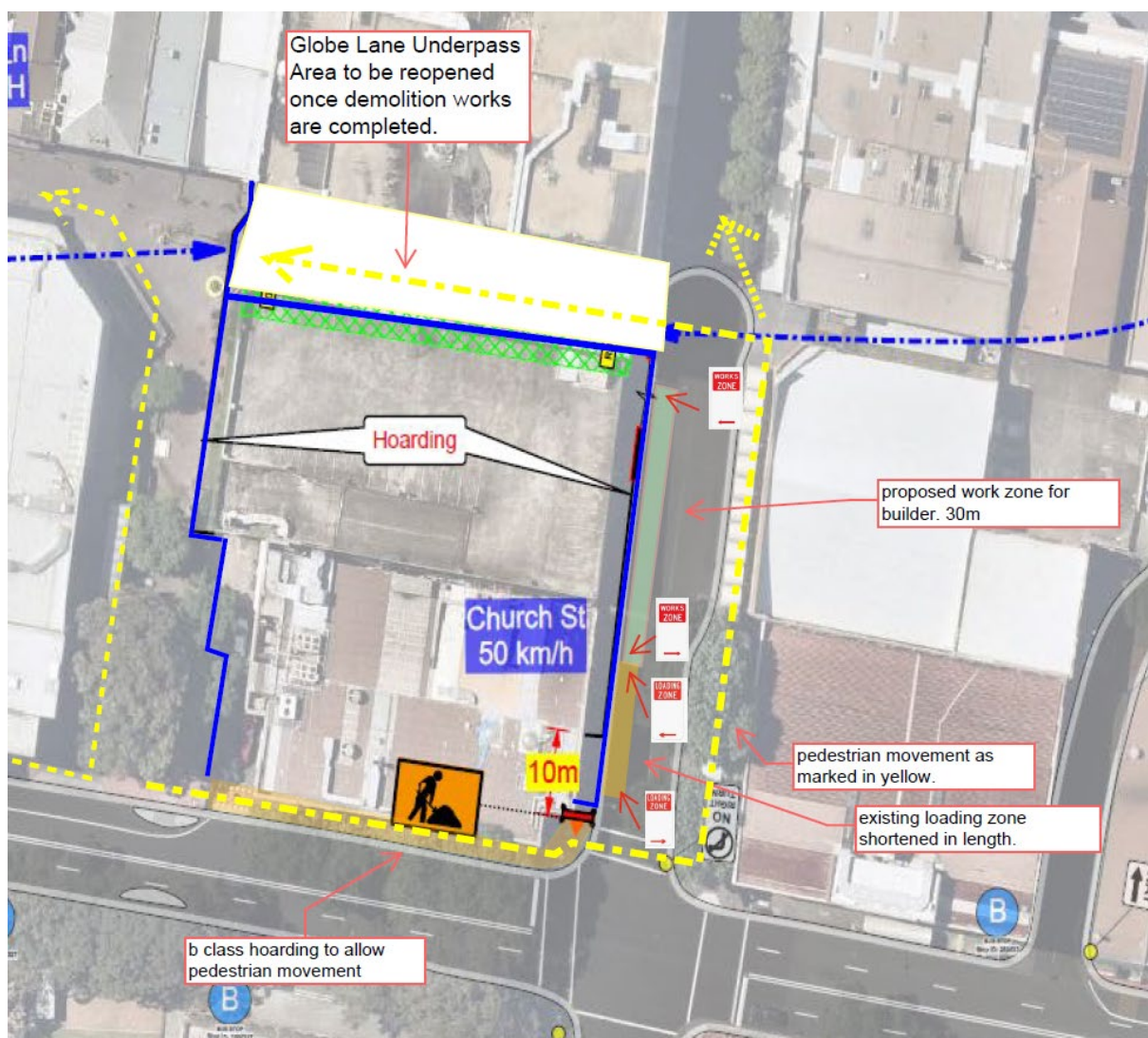


Figure 5 – Pedestrian Movement – Excavation Phase

Assessment of traffic generation with the construction of the proposed development (identifying surround constraints, business, taxi stands, proposed changes to street signage)

Overall Traffic Generation and Truck Movements

During the demolition and excavation phase there will be an increased volume of truck movements to allow for the removal of demolition and excavation materials from site, supply and removal of scaffolding and other required materials.

Figure 6 below shows the route to be used if approaching from the north.



Figure 6 – Demo and Excavation Large Vehicle Route – Approaching from North



Figure 7 – Demo and Excavation Large Vehicle Route – Approaching from South

For the Demolition phase, all demolished material will be loaded into the trucks within the site at Site access point 2 to the Globe Lane underpass area. Traffic controllers will be in place at the entry and exit points of the site. The traffic controllers will manage the movement of heavy vehicles to maintain the safety of pedestrians and road users.

A Traffic Control Plan has been be prepared as per appendix.

There will be approximately 12 truck movements per day required during the demolition phase of the project (7 months)

For the Excavation and Shoring phase, all excavated material will be loaded into the trucks at the propped work zone area on Church St using suitable sized trucks. Traffic controllers will be in place to manage loading of trucks and public/pedestrian safety.

There will be approximately 20 truck movements per day required during the excavation phase of the project (12 months)

Site deliveries and truck movements will be altered to suit bin pickup requirements in Church St as required to minimise the impact on local traffic.



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Businesses

The surrounding businesses that are to be affected by the Globe Lane Road Closure have been initially consulted regarding their requirements for general loading zones, garbage, delivery and maintenance requirements.

Further notification is to be sought with the local businesses as required once applications are made for individual items such as changes to pedestrian access, parking, loading zones and other changes due to construction activities.

Taxi Stand – Church St

The proposed construction activities will affect the traffic flow within the section of Church St that a taxi rank is operating however the Taxi Rank will be able to maintain operation for the period of these works with the exception of 2 road closures required.

The two road closures required to this section of Church St during the course of the demolition and excavation works is for the erection of the tower crane and removal of excavator from the basement once excavation is complete. Suitable notification and consultation will be provided to Wollongong Radio Cabs to offer the most suitable time to carry out these two events.

Proposed Changes to Street Signage

The existing signage to Church St at Burelli St is shown in Figure 8 below. Existing loading zone to Church St approx 40m in length. Burelli St currently consists of an existing bus stop, no parking zone and assessable parking zone as shown in Figure 8.

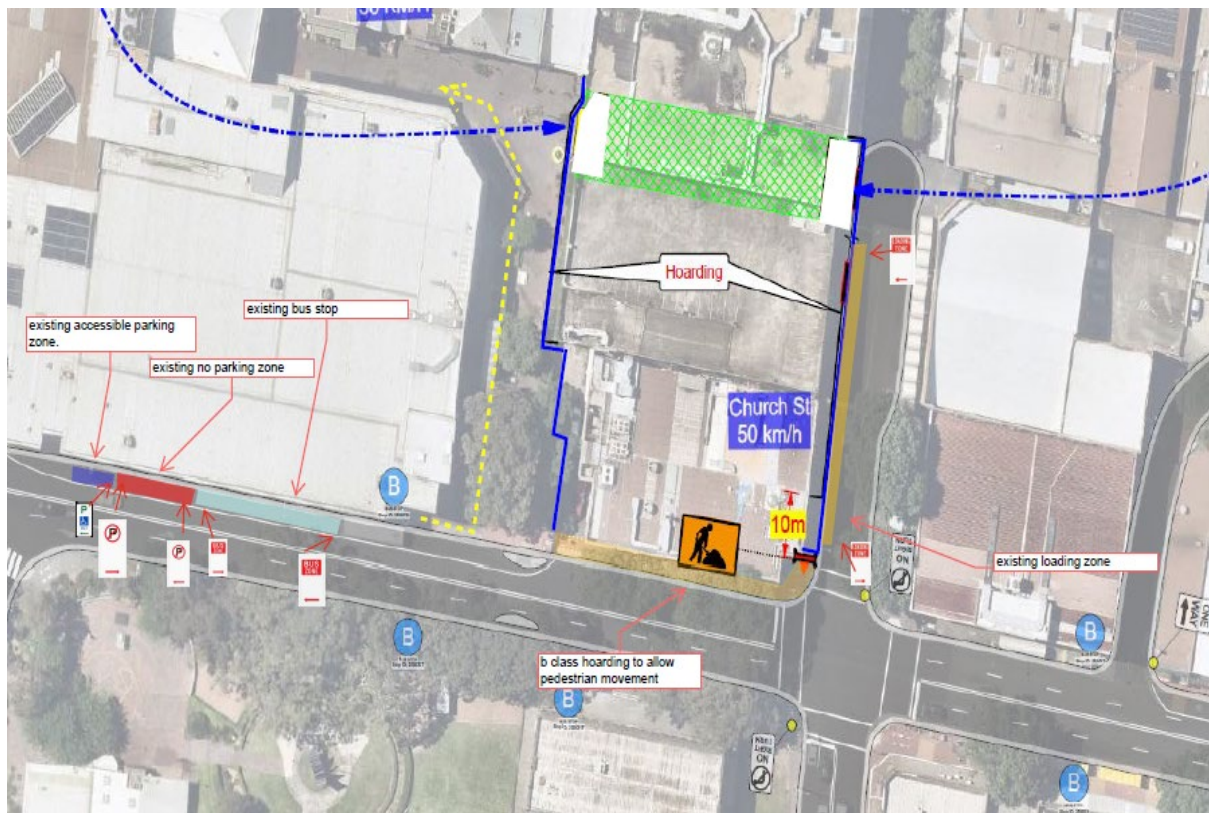


Figure 8 – Existing Signage Plan

After consultation with businesses and Wollongong Council Traffic team the proposed signage layout is shown in Figure 9.

It is proposed to have a builders work zone on Church St for 30m and the remaining 15m as a loading zone for nearby businesses.

It is proposed to establish a loading zone on Burelli St to allow for large truck deliveries for Globe Lane businesses who previously used the Globe Lane underpass area. It is also proposed to remove the no stopping zone on Burelli St to allow for the bus stop to move to the west by 10m in length.

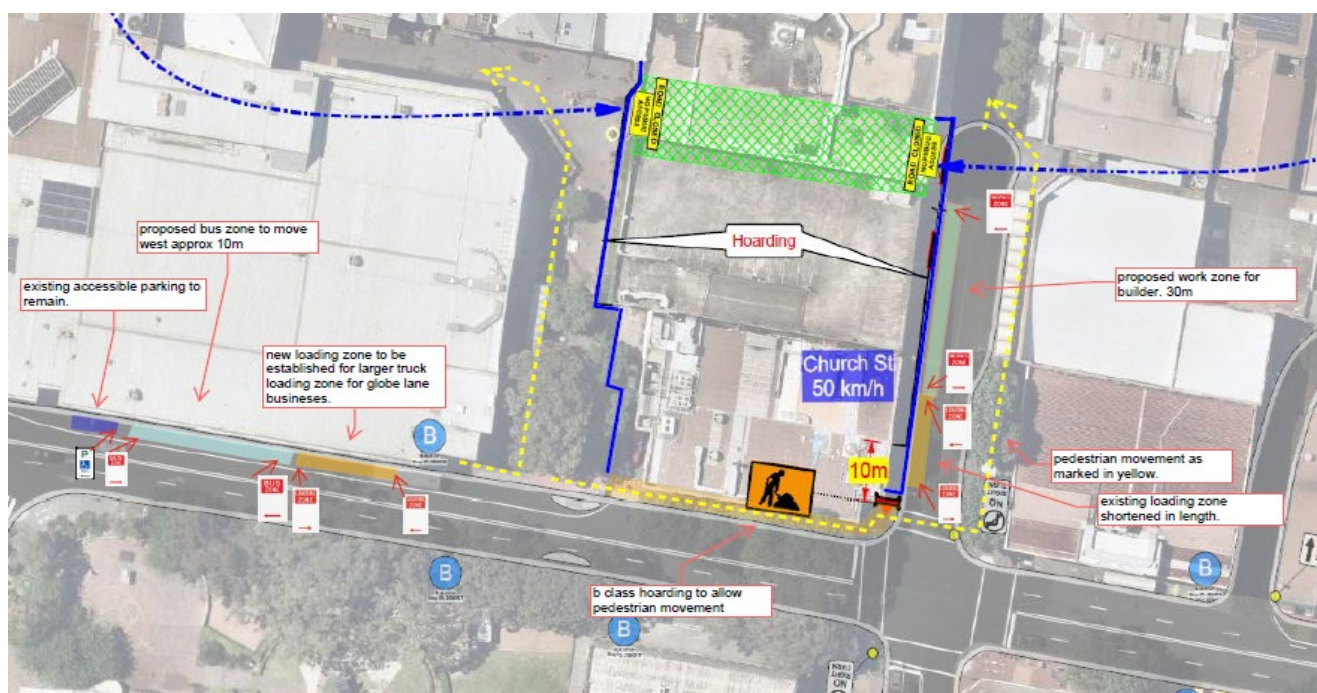


Figure 9 – Proposed Signage Plan

Proposed Traffic control measures for the demolition and excavation phase

Traffic control will be used for both the demolition and excavation phase of the development as required to safely direct trucks, pedestrians, machinery and public road users.

The current traffic guidance schemes are included in Figure 11 & 12 and will be amended as required for additional construction activities during the course of the works.

Proposed hours of the development during both the demolition and excavation phase

The proposed working hours are 7am – 5pm Monday – Saturday subject to DA requirements.

Proposed temporary parking or storage arrangements for heavy vehicles awaiting their turn to service the site

Should there be temporary parking required for trucks entering site is proposed they are parked at the southern end of Church St prior to accessing site.

Proposed car parking for construction workers

Construction workers are to use the all-day parking located at the Southern end of Church St.

Proposed location/frequency of any road closures required.

1. Globe Lane Road Closure for Demolition – 7 months (Full Closure)
2. Closure of Church St for Crane Erection – 8 hrs (Full Closure)
3. Closure of Church St for Excavator Removal (8 hrs)

Major Site Machinery

The major site machinery that will be required onsite is Tower crane, Drilling Rig, Mobile cranes and large excavation machinery.

The demolition will require a small mobile crane to lift an excavator to position for part of the demolition process. This will not require a full road closure and a traffic guidance scheme and 138 permits will be applied for to meet these requirements.

The installation of the tower crane will require an 8 hr road closure on Church St. Once this is required a traffic guidance plan will be generated and applied for through the Council Traffic Committee as required.

The float delivery and pickup of drilling rigs, and large excavation machinery will not require a road closure and can be completed after hours to limit the disruption to local businesses. Large excavators will be used with rock breaking hammers and saws to complete the excavating works on the project.

The final phase of excavation will be maintained to the Church St frontage. A Mobile crane will be required to remove the final excavator machine from the basement excavation area and this will require an 8 hr road closure to establish the mobile crane on Church St.

Proposed developer community engagement / structure during works

The builder and developer are committed to engage with the community, Wollongong City Council and local businesses as required for the duration of the project to ensure we can address the needs of the community and businesses where possible.

The builder already has a list of local businesses and contact details and will continue to engage via email notifications, signage and further consultation as required.

The builder will also continue to work closely with Wollongong Council traffic committee, Wollongong Mall authority and Wollongong Council Compliance team to ensure the community and local businesses are kept informed of the works and provide consultation before making decisions.



Figure 10 – General Site Plan

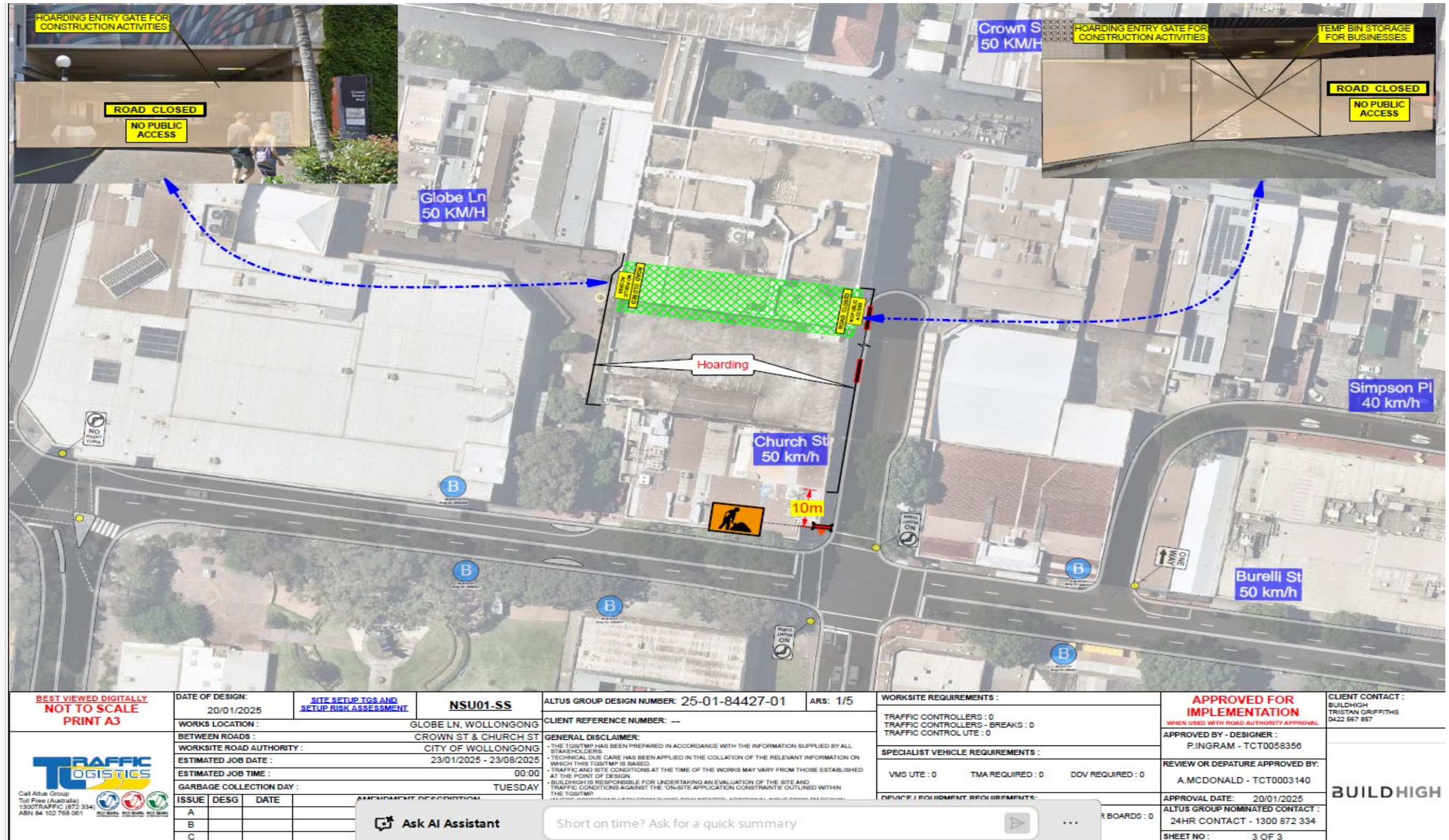


Figure 11 – Traffic Guidance Scheme – Road Closure Proposed at Globe Lane

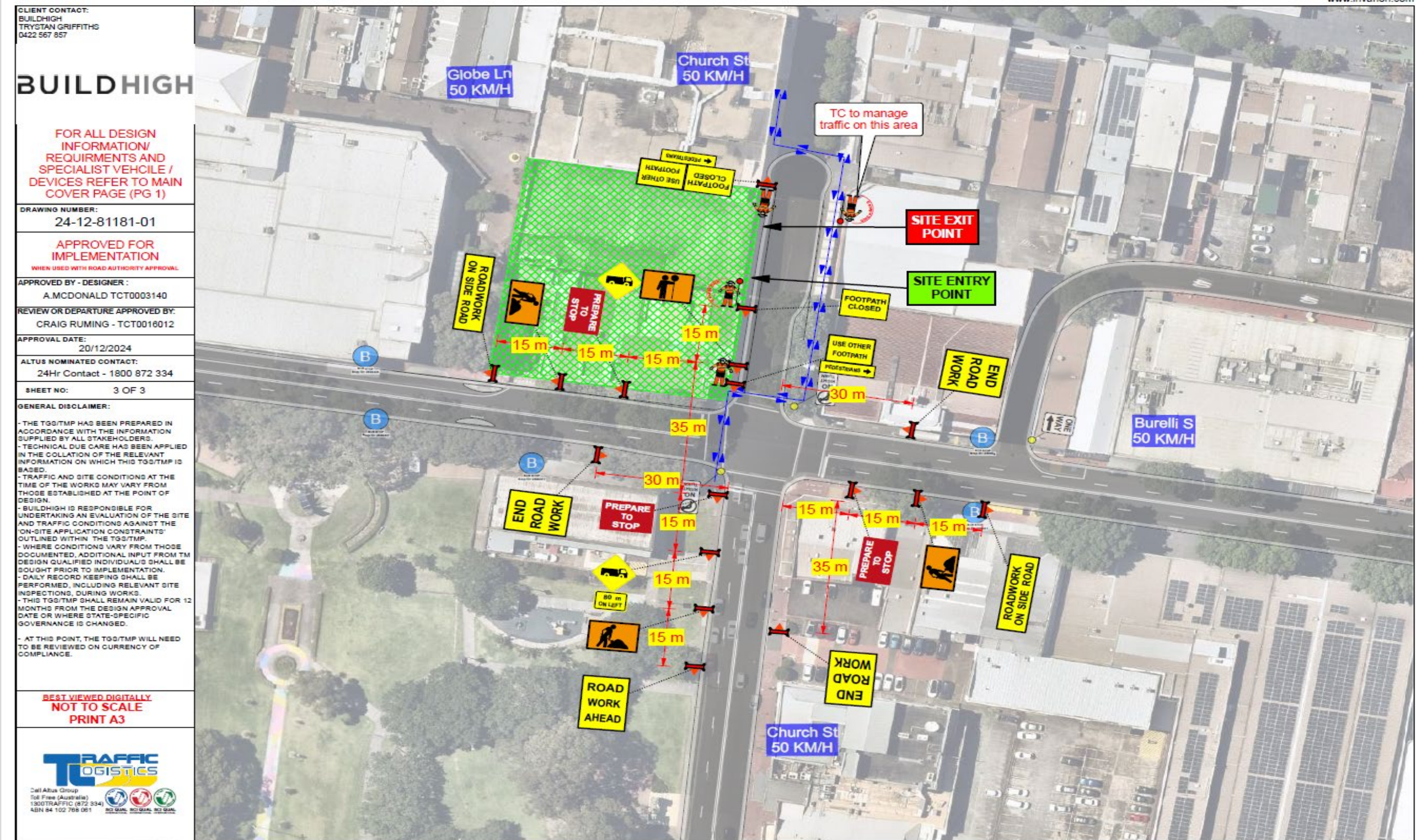


Figure 12- Traffic Guidance Scheme – Demolition and Excavation Traffic and Pedestrian Management